

## TRANSPORTATION ELEMENT



TOWN OF HOWEY-IN-THE-HILLS

LAKE COUNTY, FLORIDA

ADOPTED ON OCTOBER 11, 2010

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**TRANSPORTATION ELEMENT  
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## CHAPTER 2 TRANSPORTATION ELEMENT

### A. PURPOSE AND FORMAT

The purpose of the *Transportation Element* is to plan for future motorized and non-motorized transportation systems, pursuant to Chapter 163, Florida Statutes. An essential basis for planning transportation systems is the *Future Land Use Element*, specifically the *Future Land Use Map*. Clearly, the *Future Land Use Map* will direct where roadway facilities must be improved and where new roadway facilities may be needed. The criteria for determining the extent of facilities needed are the adopted level of service (LOS) standards.

Before a local government can responsibly plan for its future, it must assess the capability of its existing transportation system to serve current demand. It is, therefore, necessary to determine existing levels of service and to identify existing roadway deficiencies within the transportation system.

The content of this *Element* includes: (1) an introduction; (2) an inventory of the existing transportation system, including the *Existing Transportation Map*; (3) an analysis of existing roadway conditions within the transportation system; (4) an analysis of projected needs; (5) a discussion of issues and opportunities; (6) a listing of goals, objectives, and policies; and (7) the *Future Transportation Map*.

### B. INTRODUCTION

#### 1. Transportation System Overview

The Town of Howey-in-the-Hills, with a population of 1,106 (2015) is located in the central portion of Lake County adjacent to Little Lake Harris. Only three major roads provide access into Town: (1) County Road 48, (2) State Road 19, and CR 455. County Road 48 provides a direct connection to the City of Leesburg and US 27. State Road 19 provides direct access to the Florida Turnpike and the cities of Groveland (south) and Tavares (north). County Road 455 provides access into Town from the east, intersecting SR 19 just south of Town.

The historic downtown area of Howey-in-the-Hills is served by local streets in a more traditional grid system. All the streets in Howey-in-the-Hills are paved, although the pavement widths vary. Newer development in the Village Mixed Use areas typically applies a more curvilinear street design.

**C. INVENTORY OF THE EXISTING SYSTEMS**

**1. Present Town Limits**

The *Existing Transportation Map* provides a description of the Town’s current system. Table 1 lists the current streets within the Town.

As previously noted, County Road 48, State Road 19, and County Road 455 are the main routes that provide access to and from Howey-in-the-Hills. County Road 48 is a two-lane undivided collector that connects US 27 and SR 19. State Road 19 runs north and south from Groveland to Tavares at US 441. It is classified as a two-lane undivided arterial. State Road 19 is the primary north-south route through Town. County Road 455 is not in the Town limits but provides access to the south end of Town from the east.

There are a few other roads in Town that feed County Road 48 and State Road 19. County Road Number 2 connects with CR 48 west of Howey-in-the-Hills. A portion of CR No. 2 is in the Town’s Utility Service/Planning Area as it heads east towards Howey’s historic downtown area. In the Town limits, the road’s name changes to Grant/Central Avenue. Grant/Central Avenue connects with SR 19 in the Town Center and crosses SR 19 where it terminates at Lakeshore Boulevard. Florida Avenue is a local road in Town that connects Grant/Central Avenue with SR 19 on the south side of Town. And Lakeshore Boulevard/E. Revels Road connects the neighborhoods along Little Lake Harris to SR 19, again on the south side of Town.

**TABLE 1: STREETS WITHIN THE TOWN LIMITS**

Street Name	General Notes	Length in Miles
7th Avenue	Local road; intersects Revels Road	0.03
Bellisimo Place	Local road; Venezia South	0.64
Calabria Way	Local road; Venezia South	0.20
Camino Real Boulevard	Local road; adjacent to State Road 19	0.58
County Road 48	Major Collector road	0.67*
E. Camellia Way	Local road; connects to Lakeshore Dr. and N. Citrus Ave.	0.32
E. Cedar Street	Local road; connects to Lakeshore Dr. and State Road 19	0.21
E. Central Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.14
E. Croton Way	Local road; connects to Lakeshore Dr. and N. Citrus Ave.	0.32
E. Cypress Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.27
E. Gardenia Street	Local road; connects to Lakeshore Dr. and State Road 19	0.16
E. Holly Street	Local road; connects to Lakeshore Dr. and State Road 19	0.15
E. Lakeview Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.15

Street Name	General Notes	Length in Miles
E. Laurel Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.32
E. Magnolia Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.30
E. Mission Lane	Local road; connects to Lakeshore Dr. and N Valencia Ave.	0.22
E. Myrtle Street	Local road; connects to Lakeshore Dr. and State Road 19	0.16
E. Oak Street	Local road; connects to Lakeshore Dr. and State Road 19	0.17
E. Orchid Way	Local road; connects to N. Citrus Avenue and N. Tangerine Ave.	0.30
E. Palmetto Avenue	Local road; connects to Lakeshore Dr. and State Road 19	0.25
E. Pine Street	Local road; connects to Lakeshore Dr. and State Road 19	0.19
E. Revels Road	Minor Collector; connects to Lakeshore Dr. and State Road 19	0.95
Fifth Avenue	Local road; intersects with E. Revels Road	0.05
Island Drive	Local road; intersects with Lakeshore Dr.	0.25
Mare Avenue	Local road; intersects with W. Central Ave.	0.10
Marilyn Avenue	Local road; intersects with S. Marie Ave.	0.13
Messina Place	Local road; Venezia South	0.33
Napoli Way	Local road; Venezia South	0.04
N. Buckhill Road	Local road; intersects with Lakeshore Dr.	0.01
N. Citrus Avenue	Local road; connects to State Road 19 and E. Camellia Way	0.22
N. Dixie Drive	Local road; connects to W. Cypress Ave. and W. Central Ave.	0.39
N. Florida Avenue	Local road; connects to W. Cypress Ave., W. Dupont Cir., W. Oak Street, and W. Central Ave.	0.23
N. Georgia Avenue	Local road; connects to W. Cypress Ave. and W. Dupont Cir.	0.12
N. Hamlin Avenue	Local road; connects to E. Croton Way and E. Camellia Way	0.21
N. Lakeshore Boulevard	Local road: runs along Little lake Harris and connects to State Road 19	0.82
N. Tangerine Avenue	Local road; connects to Lakeshore Dr. and E. Mission Lane	0.27
N. Temple Avenue	Local road; connects to E. Cypress Ave. and E. Camellia Way	0.42
N. Valencia Avenue	Local road; connects to E. mission Lane and E. Croton Way	0.25
Number Two Road	Minor Collector; connects to S. Mare Ave. and W. Central Ave.	0.24*
Orange Blossom Road	Local road; connects to State Road 19	0.01
Palm Avenue	Local road; connects to Lakeshore Dr.	0.01
Revels Road	Local road; connects to State Road 19	0.74
San Luis Boulevard	Local road; connects to County Road 48	0.01
Silverwood Lane	Local road	0.13
Sixth Avenue	Local road; intersects Lakeshore Dr.	0.15
S. Dixie Drive	Local road; connects to State Road 19 and W. Central Ave.	0.50
S. Florida Avenue	Local road; connects to State Road 19 and W. Central Ave.	0.53

Street Name	General Notes	Length in Miles
S. Lakeshore Boulevard	Local road: runs along Little lake Harris and connects to E. Revels Road	0.97
S. Mare Avenue	Local road; connects to W. Central Ave.	0.31
State Road 19	Minor arterial; major road in Town	3.22*
Sunset Drive	Local road; intersects Lakeshore Dr.	0.01
Terracotta Terrace	Local road; Venezia South	0.30
Turn Lane	Local road; intersects State Road 19	0.02
Venezia Boulevard	Local Road; intersects SR 19 (Venezia South)	0.32
W. Central Avenue	Local road; connects to State Road 19	0.52
W. Cypress Avenue	Local road; connects to State Road 19 and N. Georgia Ave.	0.22
W. Dupont Circle	Local road; connects to N. Dixie Dr. and N. Georgia Ave.	0.24
W. Grant Street	Local road; intersects W. Central Ave.	0.03
W. Holly Street	Local road; connects to State Road 19 and S. Florida Ave.	0.14
W. Lakeview Avenue	Local road; connects to State Road 19 and S. Florida Ave.	0.14
W. Magnolia Avenue	Local road; connects to State Road 19 and W. Cypress St.	0.25
W. Myrtle Street	Local road; connects to State Road 19 and S. Florida Ave.	0.14
W. Oak Street	Local road; connects to State Road 19 and S. Florida Ave.	0.14
W. Oleander Avenue	Local road; connects to State Road 19 and S. Florida Ave.	0.14
W. Palmetto Avenue	Local road; connects to State Road 19 and N. Dixie Dr.	0.07
W. Pine Street	Local road; connects to State Road 19 and N. Dixie Dr.	0.07
<b>Total Miles</b>		<b>20.13</b>

\* This length represents that portion of the road within the current Town limits.

The above table indicates that there are currently a little over 20 miles of roads in Town.

Overall, there are about 11.5 miles combined of bicycle/pedestrian pathways in Town. A detailed inventory of the bicycle/pedestrian pathways is featured in the *Recreation and Open Space Element* as well as the Analysis of Existing Transportation System section of this *Element*.

No mass transit is currently available in the Town.

## 2. Levels of Service (LOS)

The concept of levels of service is defined as a qualitative measure describing operational conditions within a traffic stream, and their perception by motorists and/or passengers. A level-of-service definition generally describes these conditions in terms of such factors as speed and travel time, freedom to maneuver, traffic interruptions, comfort and convenience, and safety.

There are six levels of service, from A to F, with level-of-service A representing the best operating conditions and level-of-service F the worst.

Level-of-service definitions – In general, the various levels of service are defined as follows:

- Level-of-service A represents free flow. Individual users are virtually unaffected by the presence of others in the traffic stream. Freedom to select desired speeds and to maneuver within the traffic stream is extremely high. The general level of comfort and convenience provided to the motorist, passenger, or pedestrian is excellent.
- Level-of-Service B is in the range of stable flow, but the presence of other users in the traffic stream begins to be noticeable. Freedom to select desired speeds is relatively unaffected, but there is a slight decline in the freedom to maneuver within the traffic stream from LOS A. The level of comfort and convenience provided is somewhat less than at LOS A, because the presence of others in the traffic stream begins to affect individual behavior.
- Level-of-service C is in the range of stable flow, but marks the beginning of the range of flow in which the operation of individual users becomes affected by the presence of others, and maneuvering within the traffic stream requires vigilance on the part of the user. The general level of comfort and convenience declines at this level.
- Level-of-service D represents high-density, but stable, flow. Speed and freedom to maneuver are restricted, and the driver or pedestrian experiences a generally poor level of comfort and convenience. Small increases in traffic flow will generally cause operational problems at this level.
- Level-of-service E represents operating conditions at or near the capacity level. All speeds are reduced to a low, but relatively uniform value. Freedom to maneuver within the traffic stream is difficult, and it is generally accomplished by forcing a vehicle or pedestrian to “give way” to accommodate such maneuvers. Comfort and convenience levels are poor, and driver or pedestrian frustration may be high.
- Level-of-service F exists wherever the amount of traffic approaching a point exceeds the amount which can traverse the point. Operations within the queue are characterized by stop-and-go waves, and they are unstable. Vehicles may progress at reasonable speeds for several hundred feet or more, then be required to stop in a cyclical fashion. The common term for this is “stop and go traffic” and it most always refers to heavy congestion.



### **3. Traffic Accidents**

Traffic accidents within the Town are primarily concentrated along SR 19 with local hot spots being the intersections at CR 48, Citrus Avenue and Revels Road. Otherwise accidents are dispersed throughout the Town and average between 20 and 30 accidents per year.

### **4. Public Transportation**

No bus or rail service is provided to the Town. The Town will continue to work with the Lake-Sumter MPO to expand the public transportation opportunities throughout Lake County.

### **5. Rights-of-Way Acquisition and Protection**

The acquisition and preservation of rights-of-way (ROW) for future road improvements is important in planning the future transportation system for Howey-in-the-Hills. The Town shall continue to coordinate with FDOT and Lake County regarding the preservation and acquisition of ROW for state and county roads within the Town limits. As developments are planned along SR 19, CR No. 2, and CR 48, Howey-in-the-Hills will work with FDOT and Lake County to determine if right of way is needed during the development approval process.

Howey-in-the-Hills is also planning for future Town roads within the Town limits. As proposed developments are reviewed, the Town is requiring additional connections between CR No. 2 and SR 19 on the south end of Town to better disperse traffic. To the extent possible, the Town will also require connections between developments to limit the amount of traffic on SR 19 and CR 48.

## **D. ANALYSIS OF EXISTING TRANSPORTATION SYSTEM**

### **1. Functional Classification**

Functional classification is defined as the assignment of roads into systems according to the character of service they provide in relation to the total road network. The functional classification of public roads in this *Element* is based on FDOT criteria, which considers quantitative and qualitative factors such as jurisdiction, land access, route length, and trip lengths. A road hierarchy is used to identify relative importance of roads within the system, provide guidance for level-of-service and design standards, aid in establishing improvement priorities, identify maintenance responsibility, and assist in determining funding and financing policies. Based on the Roadway Functional Classifications, State Road 19 is the only Arterial, County Road 48 is the only Major Collector, and County Road Number 2 and E. Revels Rd are the only Minor Collectors currently within the Town limits. The remaining roads are classified as Local Roads.

In 2007, the Town entered into an Interlocal Agreement with the Lake-Sumter MPO, along with Lake County and all the other local governments in Lake County, to create and fund a Master Transportation Concurrency Management System Program. This approach was seen as the best way to ensure that levels of service are monitored and that necessary improvements are approached on a County-wide basis to make the best use of available funds.

Table 2 below represents the Lake County Transportation Concurrency Management System traffic counts for the roads monitored in and around Howey-in-the-Hills. These counts were performed in in 2015.

**TABLE 2: LAKE COUNTY TRANSPORTATION CONCURRENCY MANAGEMENT SYSTEM TRAFFIC COUNTS, 2015**

Road Name	From	To	No. Lanes	Functional Classification	Adopted LOS	LOS Capacity	2009		
							AADT	V/C	LOS
C.R. 48	Lime Avenue	SR 19	2	Major Collector	D	13,680	8,181	0.60	B
SR 19	Lake Harris North End	CR 48	2	Uninterrupted	C	15,100	12,900	0.85	C
SR 19	CR 48	Central Avenue	2	Arterial 1	C	14,100	8,700	0.62	B
SR 19	Central Avenue	CR 455	2	Uninterrupted	C	15,100	NA	NA	B

As part of the interlocal agreement with the MPO, as new development is proposed in Howey-in-the-Hills (either land use amendments or subdivision or site plan submittals), the landowner is required to perform a Traffic Impact Study (TIS). All jurisdictions have agreed to use the same TIS methodology in order to assist the MPO staff with making it as easy as possible to administer the concurrency management system.

Any proposed development that will impact a road segment beyond the adopted level of service standards will need to follow the Town’s *Transportation Proportionate Fair Share Program*. As development is proposed, it will need to provide adequate analysis of its impact on the road segments in Town to determine if the adopted LOS will be maintained.

**2. Constrained Facilities**

FDOT requests that local governments identify constrained roadways in their Comprehensive Plans to ensure maintenance of the operating conditions, so that significant degradation in the level-of-service does not occur. A constrained roadway is one in which adding more through lanes to meet current or future needs is not possible due to physical, environmental or policy barriers.

SR 19 is a constrained facility through the Howey-in-the-Hills historical town center. The road is not only physically constrained by current development; it would also create irreversible harm to the Town's historic downtown character to create a four-lane corridor. The majority of traffic on SR 19 in Howey-in-the-Hills is through traffic. Because of the constraints placed by all the lakes in the region, SR 19 is one of the few direct routes to get from South Lake County to North Lake County. The Town has explored a three-lane section design for SR 19 through the downtown area and ultimately rejected this design solution.

### **3. Evacuation Routes**

This section identifies the designated local and regional transportation facilities, critical to the evacuation of the coastal population prior to an impending natural disaster.

A regional evacuation transportation network that links existing county-level evacuation routes and any additional arterials/collectors in the region was provided by the East Central Florida Regional Planning Council. Based on the regional evacuation network, State Road 19 is the only road in Town categorized as evacuation route in the Statewide Regional Evacuation Study. Howey-in-the-Hills has not been subject to an evacuation order as a result of storm activity.

### **4. Parking System**

At this time, the Town does not have any significant public parking facilities other than the on-street parking at the Town Hall and Library along Central Avenue. The on-street parking serves as parking for the local commercial businesses and employees and visitors to the Town's government buildings.

### **5. Intermodal Facilities**

Intermodal facilities are those transportation networks that accommodate and interconnect different modes of transportation and serve interstate, intrastate, and international movement of goods. Some facilities considered intermodal include ports, airports, bus stations, and train terminals. At this time, Howey-in-the-Hills does not have any intermodal facilities.

### **6. Pedestrian/Bicycle System**

The pedestrian pathways, such as sidewalks, are primary located in the downtown area, along a few residential streets in the northern portion of Town, and along Lakeshore Drive. There are about 4 miles of pedestrian pathways in Town in addition to the Venezia Subdivision which is developing with sidewalks on both sides of all streets.

The bicycle pathways in Town are along State Road 19 and County Road 48. While there is no striping indicating that these are bicycle lanes, the shoulders are wide enough to classify them as bicycle lanes. There are about 8 miles of bicycle pathways in Town. A detailed inventory of these facilities is presented in the *Recreation and Open Space Element* of this *Comprehensive Plan*. The existing bicycle/pedestrian pathways in Town are featured on the *Existing and Future Transportation Maps*.

The Town has a history of support for pedestrian and bicycle planning extending back to the mid-2000's. At that time the Lake-Sumter MPO has developed a regional bike map to identify all the major bikeway facilities within Lake and Sumter County. State Route 19 and County Road No.2/Central Avenue were identified in the Lake County Regional Bike Map as regional bicycle corridors. Previously, the Town Council reviewed and approved the bike facility for S. Lakeshore Boulevard and East Revels Road as a future North/South bicycle corridor. In July 2007, the Town Council reviewed a draft bike route proposed for the regional Lake-Sumter MPO bike map. The draft map included the following streets as regional bicycle facilities:

1. State Route 19;
2. S. Lakeshore Blvd and E. Revels Road; and
3. County Road No.2.

In March of the next year, the Town Council supported the MPO bike map and endorsed the bike map for final approval by the Lake-Sumter MPO Board. In addition to the map, the Council also supported the need for future bicycle improvements. These enhancements included the following:

- widen and add pavement for bike lanes;
- bicycle signage and striping;
- bicycle racks and benches (rest areas);
- regional bike maps need to identify S. Lakeshore Blvd and East Revels Road; and
- apply for regional funding for these enhancements.

Subsequent to these actions Lake County adopted its initial multi-use trails plan in 2008. The plan, which is still in effect, proposes service to Howey-in-the-Hills through the Central lake Trail. This trail has not moved forward in any more specific planning other than a general corridor alignment.

In 2019 the Town has moved forward with a pedestrian and bicycle facilities master plan as required by policies in the Transportation Element. This plan provides more detail on specific sidewalk projects and examines the Central Lake Trail options including routes preferred by the Town. Amendments to several policies in the Transportation element have been made to initiate implementation of the master plan recommendations.

**TABLE 3  
2019 SIDEWALK INVENTORY  
TOWN OF HOWEY-IN-THE-HILLS**

<b>EAST-WEST STREETS</b>				
<b>STREET</b>	<b>WIDTH</b>	<b>LENGTH (lf)</b>	<b>SIDE</b>	<b>CONDITION</b>
N. Citrus	5-feet	550	North	Good
E. Laurel	5-feet	1015	North	Good
	5-feet	850	South	Good
E. Magnolia	5-feet	680	North	Good
	5-feet	250	South	Good
E. Palmetto	4-feet	125	South	Fair
E. Central	5-feet	450	South	Fair
W. Central	5-feet	300	North	Excellent
	5-feet	300	South	Excellent
	5-feet	300	South	Fair
W. Oak	5-feet	300	South	Good
<b>NORTH-SOUTH STREETS</b>				
<b>STREET</b>	<b>WIDTH</b>	<b>LENGTH (lf)</b>	<b>SIDE</b>	<b>CONDITION</b>
N. Dixie	5-feet	250	East	Good
N. Lakeshore	6-feet	2350	East	Excellent
S. Lakeshore	6-feet	2550	East	Excellent
	4-feet	1700	West	Excellent
N. Palm (SR-19)	5-feet	2050	West	Good
	5-feet	2400	East	Good
S. Palm (SR-19)	5-feet	1900	West	Good
	5-feet	1225	East	Good
<b>VENEZIA SOUTH</b>				
<b>STREET</b>	<b>WIDTH</b>	<b>LENGTH (mi)</b>	<b>SIDE</b>	<b>CONDITION</b>
Bellissimo	4-feet	0.64	Both	Excellent
Calabria	4-feet	0.20	Both	Excellent
Messina	4-feet	0.33	Both	Excellent
Napoli	4-feet	0.04	Both	Excellent
Terracotta	4-feet	0.30	Both	Excellent
Venezia	4-feet	0.32	Both	Excellent

Source: TMH Consulting, Inc.

**TABLE 4**  
**2019 BICYCLE FACILITY INVENTORY**  
**TOWN OF HOWEY-IN-THE-HILLS**

<b>Facility Type</b>	<b>General Description</b>	<b>Miles</b>
Bicycle	SR 19 Right Shoulder from southern town limits to northern town limits	3.22
Bicycle	SR 19 Left Shoulder from southern town limits to northern town limits	3.22
Bicycle	CR 48 left shoulder from town limits to SR 19 intersection	0.67
Bicycle	CR 48 right shoulder from town limits to SR 19 intersection	0.67
	<b>TOTAL</b>	<b>11.53</b>

Source: Town of Howey-in-the-Hills Recreation and Open Space Element

**7. Deficiencies in Town**

Currently, there are no LOS deficiencies for roads in Town. Most of the roads have additional capacity to support growth. The primary transportation issue in Howey-in-the-Hills in the future will be SR 19. Although most of the traffic on SR 19 is through traffic that does not originate nor end in Howey-in-the-Hills, the Town understands the need to address this issue.

The Town has also addressed the issue of SR 19 with regards to new growth in the development review process. Two large planned unit developments that lie between SR 19 and County Road No. 2 were required to include collector roads within their developments that would connect SR 19 with County Road No. 2. These future roadways will allow for better distribution of traffic and prevent all trips from having to use SR 19 through the downtown area. As future growth is proposed in Howey-in-the-Hills, the Town will continue to ensure that the road network provides for the most efficient system and that alternative modes of transportation are encouraged. The Town’s emphasis on mixed use developments and the redevelopment of the Town Center to a live-work environment will also help to alleviate traffic on SR 19 and the road network overall.

The Town does not have its own road impact fee; it collects road impact fees on behalf of Lake County. Lake County has impact fee districts and each year, a 5-year program is approved by the County Commission that includes projects by district. Lake County does provide for input from the cities and towns in the County as to what projects receive funding; however, the final decision is made by the County Commission.

## 8. New Facilities or Expansion

The Lake-Sumter MPO has identified the regional need to improve the road network's capacity as well as maintenance concerns such as the need to replace the Little Lake Harris Bridge. The long-range plans are to widen both the bridge and County Road 48 for future capacity needs.

In 2009, the Lake-Sumter MPO and FDOT District 5 completed a comprehensive Efficient Transportation Decision Making (ETDM) study to review and identify the regional corridors for future widening within Central Lake County. This study reviewed the following regional corridors: State Route 19, County Road 561 and County Road 48. State Route 19 is currently identified as a constrained roadway through Howey-in-the-Hills. Based on this constraint, SR 19 through the Town is identified in the adopted cost-feasible *Transportation 2040* long-range transportation plan of the Lake-Sumter MPO as a two-lane restricted arterial corridor.

The Town has identified several key intersections along State Road 19 which may eventually need traffic signals as developments receive approvals and put new traffic on the roads. These intersections have been identified in a cumulative traffic study completed in 2007 for several large residential developments within the Town. These intersections are the following:

- Central Avenue at SR 19 (existing flashing light);
- S. Florida Avenue/Venezia Development Entrance at SR 19; and
- E. Revels Road at SR 19.

The *Future Transportation Map* shows two proposed roads within proposed developments that will provide connection between County Road No. 2 and SR 19 south of the Howey-in-the-Hills downtown core. These future collector roads will enhance the road network by providing alternative routes thereby resulting in better distribution of traffic. Although environmental constraints will limit alternative north-south connections, the Town will also encourage north-south routes to provide alternatives to SR 19 in the southern region of the Town's planning area.

**E. ANALYSIS OF PROJECTED NEEDS**

This section shows the methodology used for the transportation analysis of existing and future conditions for the Town of Howey-in-the-Hills Comprehensive Plan 2035 horizon. This analysis is drawn from the 2040 long-range plan prepared by the Lake-Sumter MPO. The 2040 long-range transportation plan is rooted in part in a travel demand model (CFRPM 6.0) to forecast travel demand patterns, but it also employs other factors such as economic growth needs, population distribution patterns based on County-wide forecasting rather than local government forecasts (about 7,500 new residents countywide each year), and other limiting factors such as constrained roadways. Howey-in-the-Hills has one of the constrained roadways in SR 19 south of CR 48 to CR 455.

For the short-term, the Town has adequate road capacity as evidenced by the traffic count data presented in Table 1. Growth rates have been accelerating since 2014 as the local economy and housing development recover from the recession. Over the longer term the Town can expect increasing traffic impacts. Road improvements planned and programmed for Howey-in-the-Hills and the immediate environs are limited. The replacement of the SR 19 bridge over Lake Harris is under way and is the only funded improvement. Widening SR-19 from CR-561 to CR-48 is funded for right-of-way but design and construction remains unfunded and outside of the cost feasible plan. Plans to widen CR-48 west of SR 19 are also unfunded.

**TABLE 5: 2016 LOS CAPACITY ANALYSIS**

Road Name	From	To	No. Lanes	Functional Classification	Adopted LOS	LOS Capacity	2016		
							AADT	V/C	LOS
CR 48	Lime Avenue	SR 19	2	Major Collector	D	13,680	9,300	0.68	B
SR 19	Lake Harris North End	CR 48	2	Uninterrupted	C	15,100	13,900	0.92	C
SR 19	CR 48	Central Avenue	2	Arterial 1	C	14,100	9,100	0.65	B
SR 19	Central Avenue	CR 455	2	Uninterrupted	C	15,100	NA	NA	B

As development of the Village Mixed Use projects moves forward, the limited slate of transportation improvements will result in increased levels of congestion as a necessary outcome of the constrained section of SR 19. The Town can expect travel patterns to shift to some degree as through traffic looks for alternate routes to avoid the restrictions on SR 19.



**F. GOALS, OBJECTIVES AND IMPLEMENTING POLICIES**

**GOAL 1:** Provide a safe, convenient, efficient traffic circulation system for both motorized and non-motorized transportation modes.

**OBJECTIVE 1.1:** *Safe, Convenient, and Efficient Traffic Circulation System.* Provide a safe, convenient, and energy efficient transportation system through the establishment of minimum level of service standards and the provision of multi-modal transportation facilities with proposed road improvements.

**POLICY 1.1.1:** *Minimum Level of Service Standards.* The Town hereby adopts the following level of service standards for the below listed roadway classifications:

Road Class	AADT Level of Service
Principal Arterials: None are present	C
Minor Arterials: State Road 19	D
Major Collectors: County Road 48	D

Road Class	Peak Hour Minimum Level of Service (*)
Minor Collectors: E. Revels Road and County Road No. 2	D
Local Roadways: All roadways not classified as collectors or arterials.	D

(\*) Level of service shall be predicated on the lowest quality design hour, which shall represent the thirtieth highest hour of traffic, as determined by FDOT.

**POLICY 1.1.2.:** *Review of Proposed Developments.* The Town shall review all proposed development for compliance and consistency with the adopted levels of service using its Concurrency Management System. No development shall be approved until the concurrency management system has been evaluated to determine that

estimated impacts will not cause the level of service to decrease below the adopted minimum standard.

**POLICY 1.1.3:**     ***Transportation Impact Fees.*** The Town will work with Lake County to propose impact fee projects in and around Howey-in-the-Hills. Any road impact fee-funded projects that Lake County includes in its 5-year Road Impact Fee Program will be included in the Town’s *Capital Improvements Program* to denote future capacity enhancing improvements.

**POLICY 1.1.4:**     ***Bicycle and Pedestrian Transportation Facilities.*** The Town adopts the sidewalk master plan map and the trails master plan map as the plans for sidewalks and bicycle facilities for the Town. The Town shall work with Lake County, developers, and other possible funding agencies to implement the improvements identified in the plan.

**POLICY 1.1.5:**     ***Functional Classification System.*** The Town shall classify all roadways within its jurisdictional area according to the most current functional classification system established by the Florida Department of Transportation (FDOT).

**POLICY 1.1.6:**     ***Parking and Traffic Flow.*** The Town shall require new development to submit a site plan that provides for adequate off-street parking and safe, convenient on-site and off-site traffic flow for motorized and non-motorized vehicles.

**POLICY 1.1.7:**     ***Transportation Facility Planning.*** Planning for transportation facilities shall ensure:

- a. All streets/roads are constructed and certified to meet all Town standards;
- b. Residential street layouts avoid cul-de-sacs when possible;
- c. Residential areas are accessible to emergency vehicles;
- d. Residential streets shall have interconnections where possible to better distribute traffic;
- e. New subdivisions or developments shall address circulation, access control, off-street parking and landscaping of median strips and rights-of-way; and
- f. Design criteria for landscaping and signs along new streets/roads shall be enforced.

**POLICY 1.1.8:**     ***Plantings, Fencing, or Construction.*** No plantings, fencing or construction shall be permitted on street/road rights-of-way except

with the permission of the Town Council and based on a study and finding that no safety hazard will result. In addition, all streets will be examined for existing hazards which if discovered should be removed.

**OBJECTIVE 1.2:** *Future Land Use Compatibility.* The *Transportation Element* shall be consistent and compatible with proposed growth and development shown in the *Future Land Use Element* and *Future Land Use Map*.

**POLICY 1.2.1:** *Town Transportation Forecasting.* The Town shall utilize population, dwelling unit and employment projections obtained in the *Future Land Use Element* as data assumptions in forecasting future Town transportation needs.

**POLICY 1.2.2:** *Transportation System Improvements.* In areas designated for new growth, the Town shall determine the transportation system improvements needed prior to development approvals.

**POLICY 1.2.3:** *Consistency with Future Land Use Element and Map.* Decisions and actions the Town initiates or implements that will have an impact on the transportation system shall be consistent with the adopted *Future Land Use Map* and *Future Land Use* goals, objectives and policies of this *Plan*.

**POLICY 1.2.4:** *Future Transportation Map.* All transportation planning shall be consistent with the *Future Transportation Map* which is adopted with this *Plan* by the Town Council along with the *Future Land Use Map* and the *Capital Improvements Element*.

**POLICY 1.2.5:** *Conflicts with the Future Land Use Map.* Any changes to the transportation system shall be reviewed for conflicts with the *Future Land Use Map*. The *Future Transportation Map* and the *Capital Improvements Element* shall be coordinated and changed concurrently if necessary.

**POLICY 1.2.6:** *Statement of Findings.* Any proposed amendments to this *Element*, to include the *Future Transportation Map*, shall include a statement of findings supporting such proposals.

**POLICY 1.2.7:** *Cost/Benefit Studies.* Cost/benefit studies shall be prepared and adopted by the Town as a technical supplement to any transportation capital improvement program.

- POLICY 1.2.8:**     ***Energy Efficiency.*** Energy efficiency shall be a consideration in any plans for improvements or expansion of the road network by the Town.
- POLICY 1.2.9:**     ***Coordinating Traffic Studies, Road Improvements, and Future Roadway Needs.*** The Town, through the Lake-Sumter MPO’s Concurrency Management System, shall coordinate traffic studies, road improvements, and future roadway needs with all the local jurisdictions in Lake County to ensure that the adopted levels of service are maintained.
- POLICY 1.2.10:**    ***Consistency with Adopted Levels of Service.*** The Council shall review all proposed developments for consistency with this *Element* and the impact upon the adopted level of service standards.
- POLICY 1.2.11:**    ***Concurrency Management System.*** The Town shall implement its Concurrency Management System to monitor impacts on the transportation system created by development and growth in Town. The Concurrency Management System shall also be used to assure integration of future land uses with available capacities on transportation corridors.
- OBJECTIVE 1.3:**    ***Right-of-way Preservation and Acquisition.*** Provide for the protection and acquisition and preservation of existing and future rights-of-way.
- POLICY 1.3.1:**     ***Building Encroachments.*** The Town shall continue to provide for protection of rights-of-way from building encroachments as well as providing for the acquisition and preservation of any existing and future rights-of-way.
- POLICY 1.3.2:**     ***Dedication of Needed Rights-of-way.*** The Town shall continue requiring dedication of needed rights-of-way from new development, through subdivision regulations and applicable local ordinances.
- POLICY 1.3.3:**     ***Encroachment of Development.*** The Town shall prohibit encroachment of development and required setbacks into established present and future rights-of-way and, within the law, require dedication of rights-of-way through development orders issued by the Town.

**OBJECTIVE 1.4:**        *Future Roadway Improvements.*        Provide transportation improvements to meet the projected needs of growth and development anticipated during the planning period.

**POLICY 1.4.1:**        *Analyzing Transportation Plans and Programs.* The Town shall annually (during the Town’s annual concurrency assessment) analyze transportation plans and programs of FDOT, ECFRPC, and Lake County to establish consistency and compatibility to plans and policies set forth within the Town’s *Comprehensive Plan*. The Town shall likewise notify these three entities of all programs and improvements, amendments to the *Comprehensive Plan*, and status of the Concurrency Management System which pertain to transportation.

**POLICY 1.4.2:**        *Monitoring the Transportation System.* The Town shall continue to monitor the available capacity of all roads identified in the Town’s Concurrency Management System. The Town shall use the most recent traffic count figures recorded by Lake County or FDOT to update roadway capacities. The Concurrency Management System shall include provisions requiring developers to perform, or pay fees for services in lieu thereof, a traffic impact study to measure proposed impacts on the current availability of road capacities as a requirement in the development review process. The Town will continue to work with the Lake Sumter MPO to coordinate the transportation concurrency management system as outlined in the interlocal agreement.

**OBJECTIVE 1.5:**        *Bicycle and Pedestrian Ways and Scenic Roadways.* Promote a system of bicycle and pedestrian ways in planning for transportation facilities and analyze roadways for scenic designations.

**POLICY 1.5.1:**        *Bicycle and Pedestrian Facilities Plan.* The Town shall implement sidewalk and bicycle facilities according to the adopted plan maps and include proposed projects in the Town’s capital improvements program as individual projects are presented for development.

**POLICY 1.5.2:**        *Coordination with Lake County.* The Town shall coordinate with Lake County on the implementation of the Lake County Trails Master Plan which is hereby adopted by reference. The Town shall seek to advance the Central Lake Trail component of the plan..

- POLICY 1.5.3:** *Provision of Bicycle and Pedestrian Ways.* The Town shall enforce the provisions established in the Land Development Regulations regarding the development review process that requires applicants of PUDs, site plans, subdivisions, and replats to provide for the needs of bicycle and pedestrian facilities.
- POLICY 1.5.4:** *Bicycle Storage Facilities.* The Town shall continue to provide bicycle storage facilities at existing and proposed Town parks and shall analyze the need to provide such facilities at other Town public buildings within the proposed bicycle and pedestrian plan. The Town shall enforce the guidelines established in the Land Development Regulations regarding the requirement of all new shopping centers, recreation areas, and other public uses to provide storage facilities for bicycles.
- POLICY 1.5.5:** *Preservation of Scenic Route Designation.* The Town shall coordinate with the Florida Department of Transportation to preserve the State’s designation of S.R. 19 as a “Backwoods Trail” to promote the roadway’s unique scenic character.
- POLICY 1.5.6:** *Bicycle and Pedestrian Walkways Connection.* Bicycle and pedestrian walkways shall connect schools, the downtown area, parks and recreational areas and should include plans for bicycle parking.
- POLICY 1.5.7:** *Infill Development Sidewalk Policy.* In developed areas of the Town not served by a sidewalk network, infill residential development shall not be required to construct sidewalk unless the sidewalk will extend an existing sidewalk or where the development will include 80% or more of a block face..
- POLICY 1.5.8:** *Automobile Emission Pollution.* The Town shall enforce the guidelines and standards established in the Land Development Regulations regarding bicycle paths and pedestrian walkways to reduce the potential for automobile emission pollution and promote the use of bicycles and walking in the Town.
- OBJECTIVE 1.6:** *Coordination with Transportation Authorities.* The Town shall coordinate traffic improvements and transportation planning activities with adjacent towns, the Florida Department of Transportation, Lake County, the Lake-Sumter MPO, and other public transportation authorities or planning groups involved in the planning construction and operation of transportation facilities and services.

- POLICY 1.6.1:** *Coordinate Traffic Improvement Plans.* Prior to scheduling any improvements to State or County roads in the *Five-Year Schedule of Improvements*, the Town shall notify and coordinate such improvement plans with the jurisdictional authority for that road. The Town shall coordinate transportation plans and improvements with the Five-Year Transportation Improvements Programs or Schedule of Improvements adopted by the FDOT and Lake County.
- POLICY 1.6.2:** *Consistency in Level of Service Standards.* Level of service standards established for state or county roads shall be compatible with the respective state and county level of service standards to the maximum extent allowed under the Florida Highway System Plan, Section 187.201 (State Comprehensive Plan), Florida Statutes, Chapter 163, Florida Statutes, and Chapter 9J-5, Florida Administrative Code. In the event the adopted level of service is not compatible with the level of service standards established for roads under either the jurisdiction of the Florida Department of Transportation or Lake County, the Town shall provide a justification in the data inventory and analysis supporting document of the *Comprehensive Plan*.
- POLICY 1.6.3:** *Intergovernmental Coordination Element.* The *Intergovernmental Coordination Element* shall be used as a guide in establishing or enhancing communication or transportation planning and problems.
- POLICY 1.6.4:** *Future Transportation Map.* The Town shall share its *Future Transportation Map* and proposed changes thereto with neighboring cities, towns, and the County and review for compatibility the traffic plans of those agencies.
- POLICY 1.6.5:** *Median Landscaping and Road Beautification.* The Town shall continue to pursue grant opportunities for median landscaping and road beautification.
- POLICY 1.6.6:** *Transportation Demand Management.* The Town shall coordinate with the County and Lake-Sumter MPO on a Congestion/Mobility Management Program to identify Transportation Demand Management strategies to mitigate peak-hour congestion impacts. Strategies may include: growth management and activity center strategies, telecommuting, transit information systems, alternative work hours, carpooling, vanpooling, guaranteed ride home program, parking management, addition of general purpose lanes,

channelization, computerized signal systems, and intersection or midblock widenings.

**POLICY 1.6.7:** *Numerical Indicators.* The Town shall coordinate with the County and Lake-Sumter MPO in the establishment of numerical indicators against which the achievement of the mobility goals of the community can be measured, such as modal split, annual transit trips per capita, and automobile occupancy rates.

**POLICY 1.6.8:** *Assumptions and Policies in the Transportation Element.* The Town shall ensure that all assumptions and policies in the *Transportation Element* are consistent or coordinated with other Plan Elements, the Lake-Sumter MPO Long-range Transportation Plan, the FDOT adopted Five-year Work Program, the long-range and short-range elements of the Florida Transportation Plan, the East Central Florida Regional Planning Council Strategic Regional Policy Plan, and the Lake County Comprehensive Plan through establishment of formal coordination mechanisms and other informal coordination mechanisms.

**POLICY 1.6.9:** *Alternative Road Bypass Route(s).* The Town shall cooperate with the County, Lake-Sumter MPO, and FDOT to identify the future conditions and road improvements necessary to develop alternative road bypass route(s).

**POLICY 1.6.10:** *Coordination with Lake-Sumter MPO and FDOT.* The Town or designated transportation consultant shall work with the Lake-Sumter MPO and Lake County on the regional transportation forecast model to include the various urban collector roadways and key intersections within Town and Lake County to help distribute the traffic.

**OBJECTIVE 1.7:** *Future Transportation Map.* The Town shall exercise control over traffic planning and changes by maintaining a *Future Transportation Map*.

**POLICY 1.7.1:** *Future Transportation Map.* All traffic planning shall be consistent with the *Future Transportation Map* which is adopted with this *Plan* by the Town Council along with the *Future Land Use Map* and the *Capital Improvements Element*.

**POLICY 1.7.2:** *Conflicts with Future Land Use Map.* Any changes to the transportation system shall be reviewed for conflicts with the *Future Land Map*. The *Future Transportation Map* and the



*Capital Improvements Element* shall be coordinated and changed concurrently if necessary.

**POLICY 1.7.3:** *Statement of Findings.* Any proposed amendments to this *Element*, to include the *Future Transportation Map*, shall include a statement of findings supporting such proposals.

**POLICY 1.7.4:** *Cost/Benefit Studies.* Cost/benefit studies shall be prepared and adopted by the Town as a technical supplement to any transportation capital improvement program.

**POLICY 1.7.5:** *Energy Efficiency.* Energy efficiency shall be a consideration in any plans for improvements or expansion of the road network by the Town.

**POLICY 1.7.6:** *Future Transportation Map Review.* The Town shall review its *Future Transportation Map* yearly in conjunction with the Monitoring and Evaluation Review.

**OBJECTIVE 1.8:** *Transportation Costs.* The Town shall establish mechanisms that will allow new growth to proportionally contribute to the cost of new transportation capital facilities.

**POLICY 1.8.1:** *Impacts on the Town's Transportation Facilities.* The Town shall review all development proposals and require that they provide sufficient information to ascertain impacts on the Town's transportation facilities.

**POLICY 1.8.2:** *Development Agreements.* Where feasible, the Town shall enter into development agreements with proposed land developments to establish how project impacts may be addressed through mechanisms such as right-of-way dedication, roadway construction, multimodal design (bicycle, pedestrian, golf cart), and impact fee payments and credits.

**POLICY 1.8.3:** *Proportionate Fair Share Ordinance.* The Town shall enforce its Proportionate Fair Share Ordinance to establish a method whereby the impacts of development on transportation facilities can be mitigated by the cooperative efforts of the public and private sectors.

**POLICY 1.8.4:** *Fair Share Payments.* The Town shall require all new developments to pay their fair share for the improvement or construction of needed transportation facilities to maintain adopted

level of services standards. Fair share payments will be collected consistent with the adopted *Proportionate Fair Share Ordinance*.

**POLICY 1.8.5:** *Proportionate Share of Cost.* Land development shall bear proportionate share of the cost of the provision of the new or expanded road capital facilities or signalization required by such development.

**POLICY 1.8.6:** *Imposition of Impact Fees.* The imposition of impact fees is a preferred method of regulating land development in order to help ensure that it bears a proportionate share of the cost of road capital facilities necessary to accommodate that development.

**POLICY 1.8.7:** *Compensation for Administrative Expense.* After compensation for administrative expense, all remaining funds collected from road impact fees shall be used for the purpose of capital improvements for the expansion of transportation facilities associated with the arterial and collector road networks in Howey-in-the-Hills or the adjacent County network.

**POLICY 1.8.8:** *Capital Improvements Funding.* Transportation capital improvements that may be funded by impact fees include transportation planning, preliminary engineering, engineering design studies, land surveys, rights-of-way acquisition, engineering, permitting, and construction of all the necessary features for arterial and collector road construction projects of the type made necessary by the new development.

**OBJECTIVE 1.9:** *Access Points.* The Town shall continue to enforce its zoning regulations to control access points onto collector streets and roadways.

**POLICY 1.9.1:** Guidelines and standards for the location of access points on County Roads shall be coordinated with Lake County and on State Roads with the FDOT (the permitting authority) with the following guidelines addressed as a minimum:

- a. Access points to major streets/roads shall be limited in number.
- b. Where frontage roads are available, no additional access points shall be permitted between established intersections.
- c. In the design of new areas, frontage roads shall be provided, whenever right-of-way is available, to provide access to private property; and

- d. Distance from intersections, width and frontage requirements should be studied.

**OBJECTIVE 1.10: *Environmental Concern.*** The environment shall be a major concern in any expansion of the transportation system.

**POLICY 1.10.1:** *Natural Environment Sensitivity.* Planning for future transportation improvements shall recognize the sensitivity of the natural environment to protect the quality of existing and future neighborhoods.

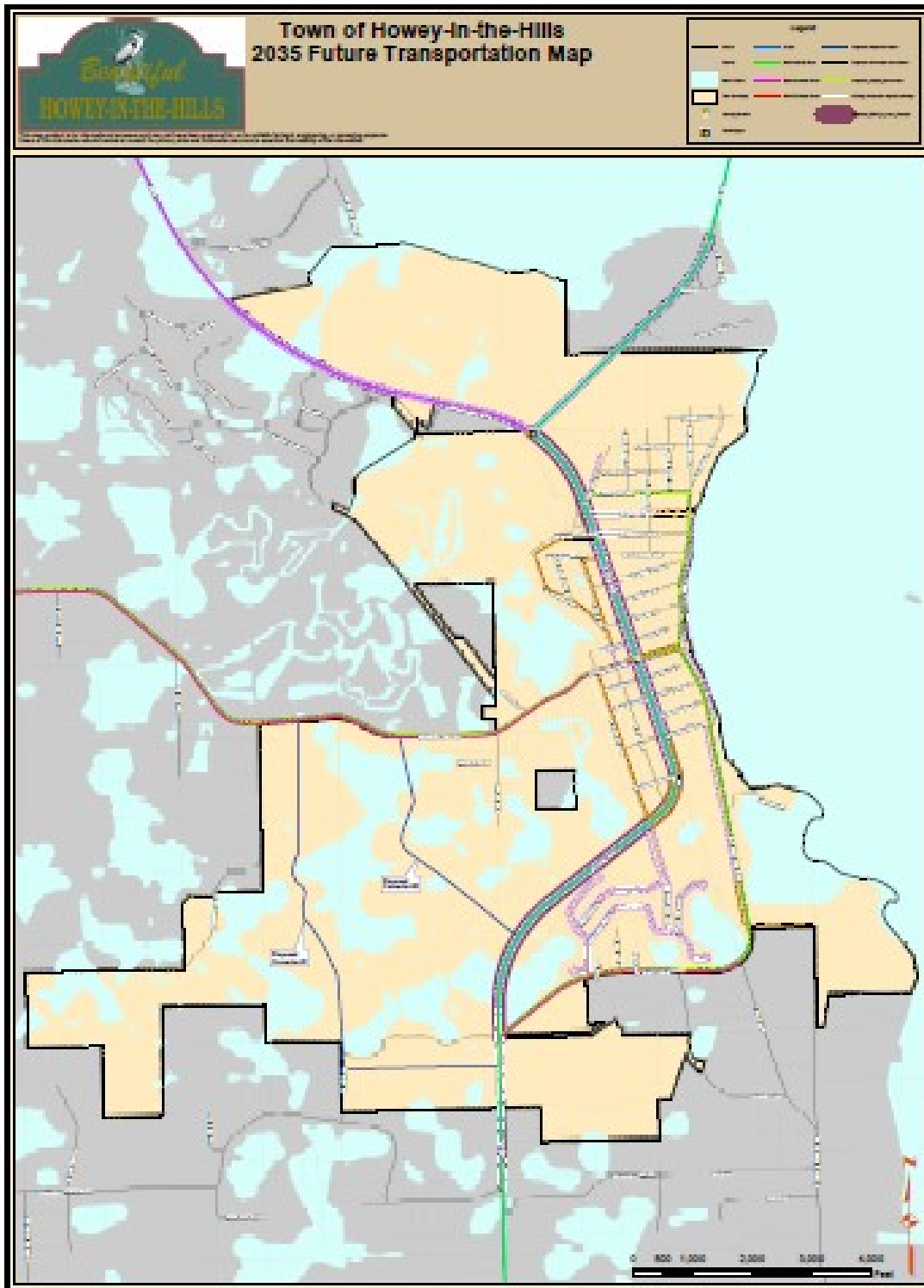
**POLICY 1.10.2:** *Conservation Resource Areas.* Transportation facilities shall not be placed in conservation resource areas or impact those places unless an overriding public need can be clearly demonstrated.

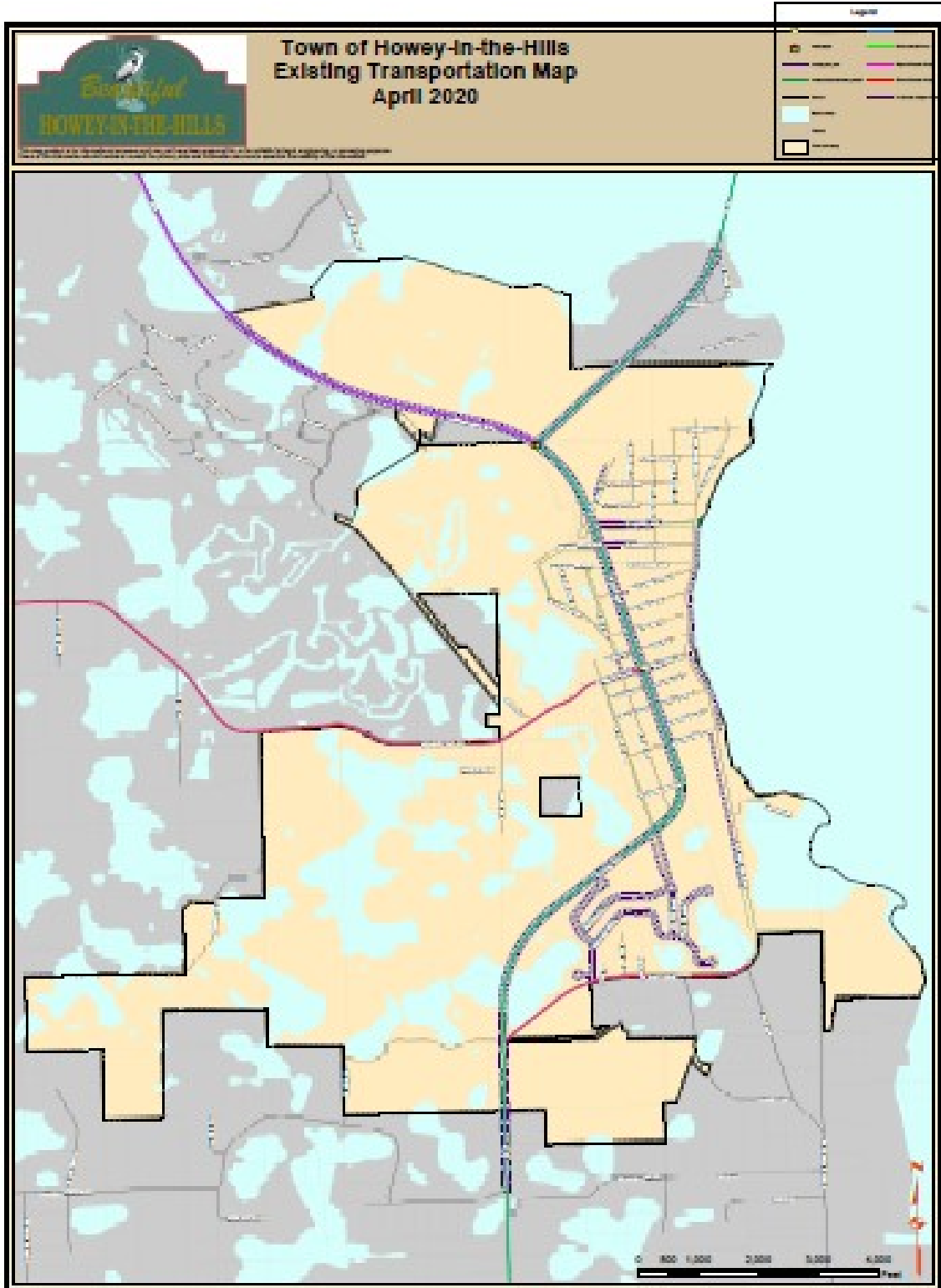
**POLICY 1.10.3:** *Encouraging the State and County.* The State and County shall be encouraged to monitor the environment before, during and after construction of traffic improvements.

**OBJECTIVE 1.11: *Public Transit.*** In the event that Lake County Express proposes any plans to establish a transit route in Howey-in-the-Hills, the Town shall work with Lake County Express to provide a safe and efficient public transit system.

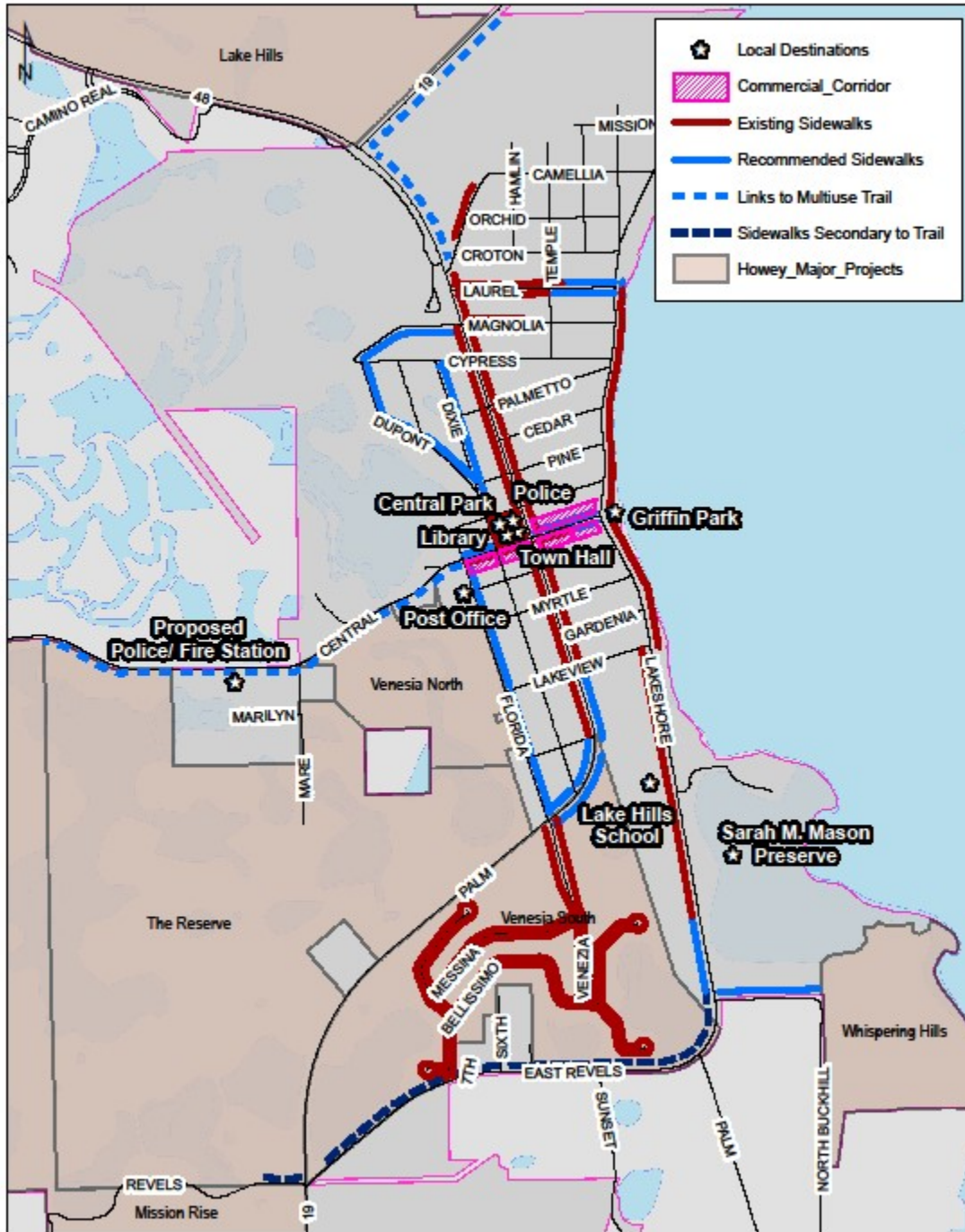
**POLICY 1.11.1:** Transit ridership shall be accommodated on certain Town roads. It is anticipated that if Lake County Express were to establish a route through Howey-in-the-Hills, it would most likely be taking residents to work, shopping, or other venues outside the Town. The Town shall encourage any such public transportation by:

- Working with Lake County Express to determine where a transit stop may be feasible.
- Requiring transit stops to meet ADA requirements; and
- Clearly delineated walkways from the building to the transit stop.





### Howey-in-the-Hills Sidewalk Master Plan Map



### Howey-in-the-Hills Trails Master Plan Map

